



The underlined sections indicate the changes compared with the previous issue.

The **Mini Classe Rules** hold all the articles unchangeable that concern especially the way the Classe Mini works.

A section called **Special Rules for Events** includes all the rules that can change from a race to another according to the length of the courses.

The **Mini Rules** with the **Production Boat Mini Rules** only includes articles on measurements. In general, what is checked during the measurement sessions.

In case of discrepancy between the French and the English version, the French version of this Guide shall prevail.

Mini Class Rules

2005 edition

FOREWORD

All the Mini events are organised under the aegis of the race National Sailing Association and under the control of the Classe Mini Association with the participation of the Organizers and local communities.

Mini events are sporting events meant to encourage offshore racing in 6,50 meters long monohull sailboats.

The objective is to encourage the study and development of these boats to increase safety whilst permitting development of performance in offshore conditions.

The rules are designed to encourage seamanship and sense of responsibility for all crews, to promote sportsmanship and solidarity between competitors.

**R-1 - RESPONSIBILITY**

R-1-a All boats must be entirely independent. Each crew must be wholly responsible for race completion. Each crew must deal with all events at sea on its own and be capable of making it to a port under its own steam.

The race courses are such that it is not possible to guarantee the success of rescue operations. The follow up of the race, put in place by the organisation, cannot guarantee the total safety of the competitors who must not expect any safety operation.

R-1-b The full responsibility of all incidents rests with the crew. The Organizers or the Classe Mini cannot be held responsible by the competitors or third party for any accident during the race or its preparation.

R-1-c Each competitor is free to abort the start or abandon the race at any time.

R-1-d In case of retirement, the competitor must do everything possible to alert the race organisation so as not to incur penalties.

R-2 - ELIGIBILITY

R-2-a Mini events are open to all monohulls complying with the Mini Classe Rules, Special Rules for Events and Mini Rules of the ongoing year.

R-2-b Any boat registered under French flag must answer one of the following requirements :

Prototypes :

According to the article 224-1.04 of the order of September 30th, 2004 concerning the safety of ships, the owners of prototypes will have to supply the affidavit of the compliance of a prototype boat (see model back page of the French guide) showing that the boat answers the requirements of the category of conception B.

However, the prototypes approved in 3rd category of navigation by the Affaires Maritimes before 01/01/2005 can answer only the requirements of the category of conception C.

**Production boats :**

Production boats launched after June 16th, 1998 must be stamped "CE" and classified in category of conception B.

Boats launched before this date must have been the object of a national approval as production boats in 3rd category of navigation.

R-2-c The Race Committee reserves the right to refuse an entry to any boat which is deemed to be in a state insufficient to withstand race conditions.

R-3 - CATEGORIES

R-3-a Inscriptions are divided up into two categories : prototypes and production boats.

R-3-b The Transgascogne is the only race where will be admitted registrations single and double-handed.

R-4 - RANKING

R-4-a There will be an overall ranking for both production boats and prototypes. A separate production boats ranking will be realised if there are more than 5 participants in this category. Classifications are set in time except for the Challenge Mini.

R-4-b Other trophies may be allocated in agreement with the Classe Mini.

R-5 - ENTRIES

R-5-a Entries are opened to all boats satisfying the eligibility criteria (Mini Rules, Special Rules for Events, Classe rules).

R-5-b Competitors less than 18 years of age must have a certificate of parental agreement.

R-5-c The last date of entry is fixed by the Notice of Race for each event. Entry forms are available from the Organizers of each event.

**R-6 - ENTRY FEES**

Entry fees are fixed in accordance with the Classe Mini.

R-7 - ENTRY ORDER AND NUMBER OF ENTRIES

R-7-a The number of entries is limited by the Organizers with agreement of the Classe Mini for each category. In case the maximum number of registrations is reached, the production boats' number will be limited to 50% of the fleet minus the competitors from a country which is not member of the European Union granted with a Derogation to the Qualification Calendar such as defined in the article R-8.

R-7-b To be considered as fully registered for an event, each competitor must have paid the entry fees, must have completed the qualifying course, if there is, and have been accepted by the Race Committee and the Jury after the prestart controls in the starting harbour.

R-7-c The entries, except DCO, will be governed in chronological order once all the requirements bellow will have been filled, without matter of the order :

- the sending date of the entry form and the entry fees (date of postmark) to the Organizer,
- the date of the end of the qualification in race (if necessary),
- the date of the end of the single-handed qualification (if necessary).

R-7-d Cases of a tie (especially for the Mini Transat) :

R-7-d-1 In case of a tie, the competitor who has sailed the greatest distance on the boat registered during official Mini events will have priority. Only events that the competitor have finished count. An event is considered as "finished" when the racer has finished the entire race (all legs of the race, if there is more than one) without any external assistance (cf : R-14).

R-7-d-2 If two competitors have sailed the same distance on the boat registered during official Mini events, the competitor who has sailed the greatest distance during official Mini events, on any boat will have the priority.



R-7-d-3 If, after this, two competitors are still in a tie, the date they have done the single-handed qualification (if necessary) course will count.

R-7-d-4 If, by chance, two competitors happen to still be in a tie, the day they had sent the check for entry fees will count, date the letter has been postmarked.

R-7-e As the chronology is so important, we take into account the following dates for the essential parts of subscription :

- Qualification in race : day of the official results proclamation of the concerned event.
- Single-handed qualification : day of the end of the course (official stamp counts).
- Entry fees check : day of the postmark.

R-8 - DEROGATIONS TO THE QUALIFICATION CALENDAR (DCQ)

R-8-a For the Transat 6,50, 6 boats maximum (either prototypes or production boats) coming from a country which is not member of the European Union will be granted with a Derogation to the Qualification Calendar.

R-8-b These competitors granted with a derogation to the qualification calendar will be selected with agreement of the Classe Mini and the Organizers.

R-8-c Competitors granted with a derogation to the qualification calendar will have to complete the qualifications as defined in the articles E-10-b and E-10-c.

R-8-d The qualifying courses for the competitors granted with a derogation to the qualification calendar shall be defined by the Classe Mini.

R-8-e Competitors interested will have to send to the Classe Mini an application file notably including the foreseen sailing program.

R-8-f The deadlines are as follows in the year of the Transat :

- Deposit of the files : March, 1st,
- Communication of the list of the selected skippers : April, 1st,
- Communication of the final list : end of the Transgascogne.



A place will be kept for the selected skippers on the registration lists until the end of the Transgascogne or the last qualifying event before the start of the Transat 6,50. After this deadline, if they have not come up with the qualification requirements, their place will be assigned to the skippers on the waiting list.

R-8-g For the Transat 6,50, maximum 3 places will be kept for the first 3 boats qualified corresponding to the prototype mini rules that have been measured for the first time the year of the Transat.

R-9 - DOCUMENTS

To be properly registered, each skipper must supply the following documents :

To the Classe Mini :

- photocopy of ship's license or equivalent,
- all the requirements detailed in R-2-b,
- copy of liferaft booklet,
- VHF license and callsign,
- Sarsat MMSI number delivered by National authority,
- VHF license (for the crew),
- EPIRB coding form,
- colour pictures of the boat, on paper (side view and upper view) (1),
- advertising card (depending of national authority),
- CV and 2 passport photos for each member of the crew,
- FFVoile license or national authority equivalent (including medical certificate) for the ongoing year,
- certificate of complementary insurance in third part liability for the amount of € 4.500.000 for any race beyond 200 NM from the coasts,
- for people who are not member of the FFVoile, a certificate of insurance in third part liability for the amount of € 4.500.000,
- Classe Mini membership (2),
- photocopy of passport,
- parental approval certificate for minors,
- onshore contact address during the event,



- measurement booklet filled by a measurer during measurement session (including ongoing year certificate of compliance to the Mini Rules).

(1) Those pictures will have to be provided for each event. They won't be given back. The pictures of the boat must represent the decoration during the specific event.

(2) For double handed races with at least one leg of more than 300 NM, both the skipper and the co-skipper must subscribe the Classe Mini.

To the Organizers :

- Entry fee,
- Entry form.

R-10 - PRE-RACE ARRIVAL

Boats and skippers must be available for the Race Committee at least :

- 10 days before the start of a race with at least one leg of more than 1000 NM non-stop,
- 4 days before the start of a race with at least one leg of more than 300 NM non-stop,
- 60 hours before the start of a race with no leg of more than 300 NM,
- 24 hours before the start of a coastal race.

R-11 - PENALTIES

R-11-a Penalties for being late regarding the pre-race arrival can only be financial and may be as follow :

- € 80 for the first day,
- € 160 for the second day,
- € 320 for each following days.

R-11-b These penalty fees will be donated to the Sea Search and Rescue Donation.

**R-12 - INSPECTION**

R-12-a Each boat must be checked in the starting harbour before being declared definitively entered. This check determines if the boat complies with all the Classe Rules and safety.

R-12-b The rejection of a boat will be notified to the crew as early as possible to allow crew to put things right. If time permits, a second check will be carried out.

R-12-c Boats may be checked at any time.

R-12-d The Measurement Commission may perform any measurement in order to assess seaworthiness and safety of the Minis.

R-13 - SEALING OF EQUIPMENT

Competitors are responsible for the proper handling of seals and the reliability of the fixing points used.

R-14 - OUTSIDE ASSISTANCE

R-14-a No kind of outside routing is allowed. Each competitor will have to sign a declaration committing himself to respect this rule.

R-14-b The VHF is the only communication system authorised aboard during the events (mobile phone, satellite phone, computer... are strictly forbidden).

R-14-c Mobile phones can be embarked provided that they are in a specific sealed container for the duration of the event. Any unseal implies a definitive expulsion of the race.

R-14-d Any infringement of those rules will have the following consequences : life ban from the Classe Mini and inquiry of this infraction to the National Sailing Association.

R-14-e Any demand of assistance at sea, other than medical, excludes automatically the racer from the event. No physical contact with another boat and no provisioning, even medical, can happen at sea during the event.



R-14-f Boats can stop anywhere, anytime. The duration of the stopovers is limited to 72 hours.

R-14-g Once moored or docked in a harbour, outsiders may come aboard, and supplying and repairs may take place.

R-14-h Boats may be towed only within 2 NM from a harbour and the tow must not take the boat closer to the finish line.

R-15 - REPLACEMENT OF MATERIAL

In case of material failure, replacement of material can be made with authorisation from Race Committee and Jury who will apply time penalties governed by the Sailing Instructions.

This article concerns the following material : spars, sails, appendices (except the keel) and more generally, any material that would need another control (measure, use-by date, coding...).

R-16 - PROTESTS

R-16-a A Protest Committee is constituted during the full event. The chairman will be agreed by the race National Sailing Association.

R-16-b Protests regarding eligibility must not be lodged later than 24 hours before the start of the race. Further this delay, protests won't be admissible.

R-17 - REPLACEMENTS

R-17-a Substitutions of skippers are only granted if place is available and if substitutes are suitably qualified.

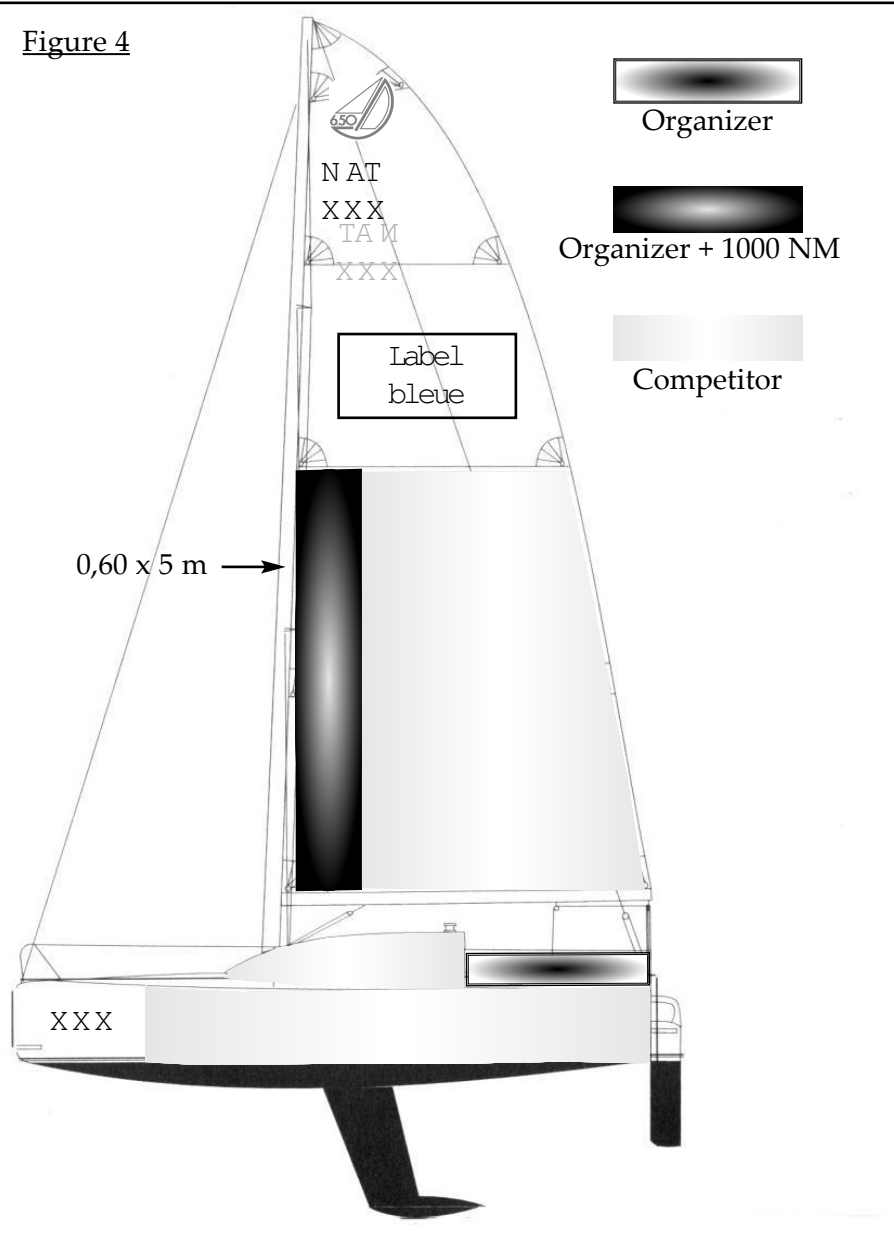
R-17-b Substitution must be agreed by the Race Committee and the Jury.

R-18 - ADVERTISING

R-18-a Advertising on boats is governed by R.C.V. 2005-2008 of ISAF rule number 79 and Appendix 1.



Figure 4



R-18-b In agreement with the FFVoile and the Organizers of the Mini races, the advertising spaces are divided up as shown in figure 4.

R-18-c The Classe Mini may ask for place for their own sponsors.

R-18-d Race numbers and ensigns must stay clearly visible at any time.

R-18-e Flags, banners and any other publicity material can only be worn in harbour, quayside or at anchor, subject to the Notice of Race of the event.

R-19 - ENVIRONMENTAL PRESERVATION

Every skipper must keep his rubbish on board up to the arrival ashore.

R-20 - EXCLUSION

The Classe Mini may ban a member who has knowingly twisted the spirit or the wording of the Mini Rules or the Mini Class Rules.

R-21 - NOTICE OF RACE

Each event has its own Notice of Race with agreement of the Classe Mini. This Notice of Race will include a minimum of items as follows :

- Organisation
- Event schedule
- Dates
- Race course
- Entry date
- Entry fee
- Time Limit
- Prize giving

R-22 - PRIZES

Prizes offered to competitors cannot be financial, if possible they are for assisting the competitor with the realisation of his Mini project.



Special Rules for Events

2005 edition

E-1 - CONFIGURATION

The boats must stay within their original configuration at the start of a race (spars, appendices, structure, sail plan) for the entire event. The crew must have on board all the safety equipment defined below in articles E-2 to E-11 for the entire race and must maintain its operational state.

E-2 - LIFERAFT

E-2-a A self-inflatable life raft complying with at least the French standards of the Class V (1975), the standard ISO 9650 F- DIS or the standard ISO 6185-1.

E-2-b It must have a valid certificate covering the entire event (date of closing of the finish line).

E-2-c It must be accessible whatever the position of the boat, from inside and from outside for the boats with an emergency hatch.

E-2-d The liferaft must be securely fastened.

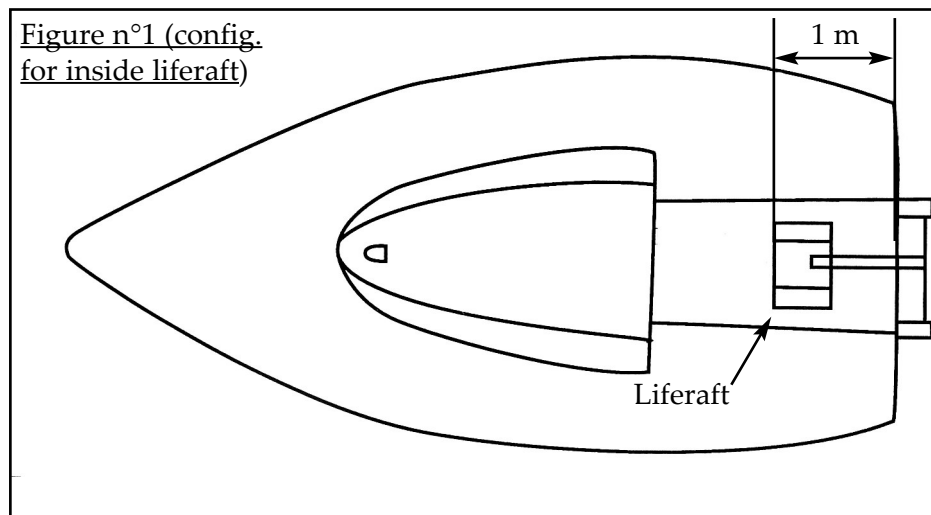
E-2-d-1 Inside, in an area from the transom to 1 meter in front of it, for boats equipped with waterproof hatches on the transom (cf: figure 1).

E-2-d-2 Outside, on the border of the transom, free from all obstacles, for all the boats built before January, 1st 1995, that did not install a waterproof hatch on the transom.

E-2-e The strap which inflates the liferaft must be fixed properly to the boat.

E-2-f The Organizer can seal the liferaft at the start of the race.

E-2-g A floating knife must be situated close to the liferaft.



E-3 - SURVIVAL CONTAINER

E-3-a A dedicated waterproof and rigid container, marked SURVIVAL and with the boat number containing the security equipment defined in the paragraph E-8.

E-3-b The survival container can be sealed before the start after inventory check and must arrive intact at the end of each race.

E-3-c Penalty shall be given to all unsealed containers.

E-4 - SURVIVAL WATER JERRYCAN

E-4-a 8 litres of fresh water, suitable for drinking, in a 10 litre jerrycan, marked SURVIVAL and with the boat's race number.

E-4-a-1 This jerrycan can be sealed at the start.

E-4-a-2 This jerrycan must be intact at the end of each race.

E-4-a-3 Competitors are responsible for its conservation.

E-4-b The jerrycan, defined in E-4-a, can be replaced by 8 litres of survival water in bags stocked in a specific sealed container marked SURVIVAL.



E-5 - DISTRESS BEACON

E-5-a A 406 Mhz and 121.5 Mhz Epirb Sarsat-Cospas beacon. The MMSI number is to be asked to your own national authority.

E-5-b It must be fixed so as to be accessible both from inside and outside the boat.

E-5-c A knife must be positioned within immediate reach.

E-5-d The beacon must be coded for the ongoing year in the name of the boat and the owner. The beacon cannot be transferred from another boat or to another boat without adapting the MMSI number.

E-5-e The beacon and the batteries must be valid for the duration of the event.

E-6 - WHITE FLARES

E-6-a Two white signal hand flares are compulsory.

E-6-b They must be stored near hatchway and be accessible both from inside and outside the boat.

E-7 - SURVIVAL SUIT

A survival suit waterproof thermal protection (permanent wearing) per crew, in accordance with the standard ISO 15027-1, category A, guaranteeing a minimal thermal protection of 0,75 clo immersed. These indications must be resumed on the label of the manufacturer.

E-8 - SAFETY EQUIPMENT

SAFETY MATERIAL	Day races	Race with no leg of more than 300 NM	Race with at least one leg of more than 300 NM	Race with at least one leg of more than 1000 NM
LIFERAFT <u>such as defined in article E-2-a</u>	yes	yes	yes	yes
Floating knife close to the liferaft	no	yes	yes	yes



SAFETY MATERIAL	Day races	Race with no leg of more than 300 NM	Race with at least one leg of more than 300 NM	Race with at least one leg of more than 1000 NM
SURVIVAL CONTAINER	no	yes	yes	yes
1 knife	no	yes	yes	yes
Survival food (min 500g for each crew)	no	yes	yes	yes
1 signaling mirror	yes (aboard)	yes	yes	yes
1 waterproof torch	no	yes	yes	yes
Fishing equipment	no	yes	yes	yes
<u>3 parachute flares</u>	yes (aboard)	yes	yes	yes
<u>3 automatic red hand flares</u>	yes (aboard)	yes	yes	yes
2 floating smoke signals	yes (aboard)	yes	yes	yes
1 handheld VHF (waterproof or in bag)	no	yes	yes	yes
1 survival blanket (for each crew)	no	yes	yes	yes
<u>3 lightsticks</u>	no	yes	yes	yes
<u>1 seamark dye marker</u>	no	yes	yes	yes
SURVIVAL WATER JERRYCAN	no	no	yes	yes
EPIRB	no	no	yes	yes
<u>Survival suit such as defined in article E-7</u>	no	no	yes	yes
COLLISION WHITE HANDFLARES	no	yes	yes	yes
DRYING - BILGE PUMP	yes	yes	yes	yes
RADIO				
1 fixed 25 watts VHF transceiver	yes	yes	yes	yes
1 topmast VHF antenna	yes	yes	yes	yes
1 emergency antenna	no	yes	yes	yes
ELECTRICAL POWER				
12 volts batteries of 200 A/h	yes	yes	yes	yes
Charge system	no	no	yes	yes
NAUTICAL BOOKS				
<u>International Regulations for Preventing Collisions at sea</u>	yes	yes	yes	yes
International Signals Code	yes	yes	yes	yes
1 Log Book	yes	yes	yes	yes
<u>Tide Tables</u>	yes	yes	yes	yes
Nautical instructions	yes	yes	yes	yes
Light book	yes	yes	yes	yes
Charts	yes	yes	yes	yes

(1) harness and lifejacket can be replaced by a lifejacket/inflatable harness conforming the European norms with a minimum flotability of 150 Newton



SAFETY MATERIAL	Day races	Race with no leg of more than 300 NM	Race with at least one leg of more than 300 NM	Race with at least one leg of more than 1000 NM
SAFETY EQUIPMENT				
1 anchor 6 Kg minimum if made of steel or 1.8 Kg if made of aluminium alloy	yes	yes	yes	yes
- with 8 m of chain diameter 6 mm	yes	yes	yes	yes
- with 25 m rope of 10 mm diameter	yes	yes	yes	yes
System allowing you to be detected by a radar	yes	yes	yes	yes
1 life jacket type BSA 55 kg or conforming to the normal European standard for each crew (1)	yes	yes	yes	yes
1 safety harness for each crew, EN 1095 standard(1)	yes	yes	yes	yes
1 fog horn	yes	yes	yes	yes
1 first aid kit such as defined in article E-9	yes	yes	yes	yes
1 horseshoe buoy fixed outside	yes	yes	yes	yes
- with 1 sea anchor	yes	yes	yes	yes
- with 1 automatic light	yes	yes	yes	yes
1 fire extinguisher type B21 or better, accessible from the outside, protected from tampering	yes	yes	yes	yes
2 x 9 litres (at least) rigid buckets with lines	yes	yes	yes	yes
1 water bailer	yes	yes	yes	yes
2 waterproof torches	yes	yes	yes	yes
1 strobe light for each crew	no	yes	yes	yes
1 corrected compass	yes	yes	yes	yes
1 hand-bearing compass	yes	yes	yes	yes
1 navigation ruler	yes	yes	yes	yes
1 SSB / HF receiver	VHF OK	VHF OK	SSB	SSB
1 log (speed/total nm)	yes	yes	yes	yes
1 depths measuring apparatus	yes	yes	yes	yes
2 accurate watches/clocks	only one	only one	only one	yes
1 barometer	yes	yes	yes	yes
1 sextant	no	no	no	yes
Tables	no	no	no	yes
1 pair of binoculars or monocle	yes	yes	yes	yes
1 boat hook	yes	yes	yes	yes
1 tricolour light visible with sails up	yes	yes	yes	yes
Tools	yes	yes	yes	yes
1 system allowing to liberate the rigging	yes	yes	yes	yes
1 oar	yes	yes	yes	yes
- with rowlock fixed to the transom	yes	yes	yes	yes
A set of Q, N, C, national and courtesy flags	yes	yes	yes	yes
1 sea anchor of minimum diameter 50cm, minimum length 1m equipped with a swivel	no	no	yes	yes
SPARE : - - - batteries	yes	yes	yes	yes
- bulbs	yes	yes	yes	yes
- fuses	yes	yes	yes	yes
Keel and rudders painted with fluorescent orange	no	no	no	yes



E-9 - FIRST AID KIT

The new French rule imposes two types of first aid kit : one for a distance up to 200 NM from a shelter and the other one beyond 200 NM. The lists of these medical subsidies can be consulted on the class website : www.classemini.com.

E-10 - QUALIFICATION COURSE

Following the Mini Transat 1999 - where about half of the fleet gave up and very many beacons were activated - the Classe Mini, strongly incited by the French Sailing Authority (FFV), had to react.

We thus set up a qualification in two steps : 1000 NM single-handed on an imposed course and 1000 NM in race, single or double-handed. That is to say, on the whole, 2000 NM carried out by the couple skipper/boat which wishes to take the start of the Mini Transat.

The objective is simple : to allow the candidates to the Transat to become aware of their true level and at the same time of their psychological aptitude for single-handed navigation.

To cross the Atlantic single-handed is not an easy navigation. It is a true personal engagement in an experience where one completely takes charge of his destiny. In the middle of the ocean, you cannot count on anybody. And the qualifications, in particular the imposed course, are there to prepare yourself.

The voluntary members of the Qualification Committee who study your qualification file, are not there to sanction but accompany your preparation.

Some see in this "qualif." an obstacle moreover on the way full of pitfalls which leads to the start of the Mini-Transat. Others, the majority of those who made it, appreciated. One learns much during his qualification. Often, one comes back changed. You will see, you will not regret it.

**E-10-a** Competitors will have to respect the following qualification rules :

Necessary Qualification	Day races	Race with no leg of more than 300 NM	Race with at least one leg of more than 300 NM	Race with at least one leg of more than 1000 NM double-handed	Race with at least one leg of more than 1000 NM single-handed
1000 NM in Mini events	no	no	no	yes (1 person only)	yes
1000 NM course single handed	no	no	no	no	yes

E-10-b Description of the 1000 NM course in Mini events :

E-10-b-1 Distance : 1000 NM cumulated in races of the Mini program, single or double handed with at least a 500 NM leg non-stop, on the boat the skipper wants to qualify with. Only events where the competitor has finished will count. However, the skipper must have finished at least one event on his boat (except day races) of the Mini program the year of the race he wants to qualify. The date of this race is not taken into account in the chronology of the order of inscription. An event is considered as "finished" when the racer has finished the entire race (all legs of the race, if there is more than one) without any external assistance (cf : R-14).

E-10-c 1000 NM course single handed :

E-10-c-1 Distance : 1000 NM single-handed on the boat the skipper wants to qualify with.

E-10-c-2 Deadline : no less than 3 months before the start of the race for which he qualifies.

E-10-c-3 This mandatory qualifying course, defined by the Classe Mini is established for each area (Atlantic, Mediterranean). This course is a loop that the competitor has to complete once. Entry point to the loop may be determined according to the starting harbour.

E-10-c-4 This course must be completed with no stop over.

E-10-c-5 This qualification course being designed to improve seamanship of the competitors, they can, in case of severe weather conditions only, decide to make a stop over.



E-10-c-6 The competitor must send to the Classe Mini by phone, email, fax or any other kind before his start the form to inform the Classe he is leaving (this form is available on the official website www.Classemini.com or can asked for at the office).

The competitor must by any kind at his disposal (onshore or offshore phone, VHF or visual signs, fishing vessels or cargo ships, semaphores) try to make know his progress.

Rounding marks must be photographed with the skipper and/or the boat clearly identifiable.

The logbook of this qualifying course must be signed by a local authority at the start, stop over (if necessary) and finish. The logbook and its associated charts be sent the Classe Mini within a fortnight (postmark date).

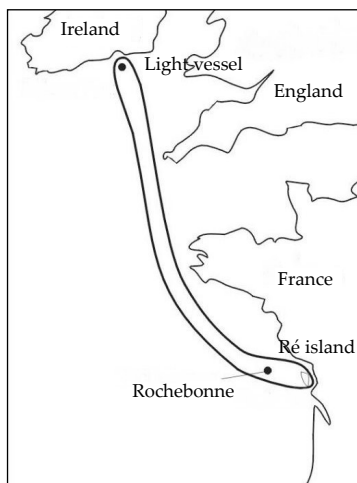
In this log-book, one has to mention :

- celestial navigation observations and calculations detailed and careful for at least two earth positioning obtained by the method line of position + meridian, or in the absence of possible observation with the zenith position, by two lines of position ; the geometrical construction of the navigation is required,
- the weather reports, especially those justifying a possible stop over,
- the following observations (8 times a day) : hour, position, compass course, log, weather observations (sea, force and direction of the wind),
- sails and changes of sails,
- any relevant observations (special marks, damages, etc...).

E-10-c-7 Atlantic qualification course :

Three rounding marks :

- Conninberg Light Vessel : 52°02 N - 6°39 W
 - Rochebonne :
 - NW buoy : 46° 12.9 N - 2° 31.9 W
 - SW buoy : 46° 10.1 N - 2° 27.0 W
 - SE buoy : 46° 09.2 N - 2° 21.2 W
 - Ré island
- (Positions non official - see figure next page)

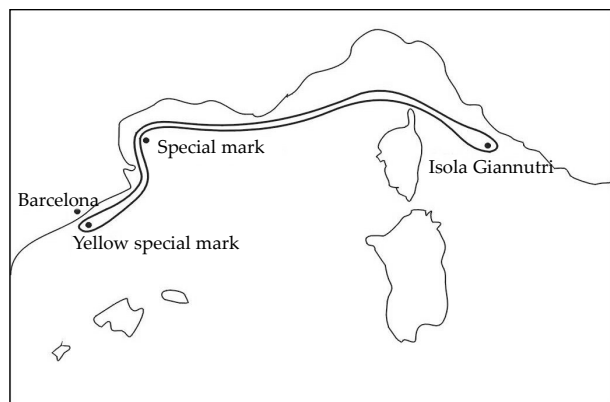


E-10-c-8 Mediterranean qualification course :

Four rounding marks :

- Barcelona : yellow special mark 41° 20.1 N - 2°13.3 E
- Golfe du Lion : Special mark buoy 42° 59 N - 3° 45 E
- Giraglia Island : 43° 01.6 N - 9°24.4 E
- Isola Giannutri : 42°14.3N - 11°06.6 E

(Positions not official - see figure below)



E-10-d Skippers who have already finished the Mini Transat single-handed are exempted from the qualifying course defined in E-10-c.

E-10-e Configuration of the boat for the qualifying course.

- During an event : boats measured, complying with the Classe Mini Rules and Mini Rules.
- Outside an event : boats complying with the Classe Mini Rules and Mini Rules.

NB : we strongly advise competitors to bring aboard the EPIRB beacon and the life suit for the 1000 NM course single handed.

E-10-f Which boats need to complete a qualifying course according to the articles E-10-a, E-10-b, and E-10-c : the boats who have never been measured, boats measured before 1995 and boats that have been modified on the following points :

- change of keel bulb and keel fin,
- change of keel structure or keel position,
- modifications calling into question the aptitude of the boat to be conformed with the criteria of stability defined in the Mini Rules,
- modifications calling into question the structural integrity of the boat, including the additions of mass in the bulb.

E-11 - RADIO

E-11-a Each boat must be equipped with a permanently installed VHF radio with an emission power of 25 watts (manufacturer specifications). A demand has to be asked to your national authority.

E-11-a-1 The real power must be at least 15 watts (as measured during the safety controls).

E-11-a-2 The antenna must be fixed at the masthead and the steady wave rate (TOS) must be lower than 1,3.

E-11-a-3 Compact antennas are not allowed because they do not carry enough. The only allowed antennas are those of 1/2 wave length (the length must be equal or superior to 95cm).

E-11-a-4 The diameter of the antenna wire must not be less than 4 mm.



E-11-a-5 An emergency antenna is required (a long antenna is recommended because they carry better).

E-11-b Any kind of radio assistance to participants is prohibited except for medical purpose.

E-11-c All radio communications must be in plain language.

E-12 - ELECTRICAL POWER

E-12-a Electrical power is provided by one or more batteries with a minimum total storage capacity of 200 amp/hours at 12 V.

E-12-a-1 The batteries must be waterproof. Waterproof means that the acid does not leak immediately when the battery is laid down.

E-12-a-2 They must be securely fastened for the whole event.

E-12-a-3 They must be fixed and stay in place regardless of heel angle.

E-12-a-4 They can be sealed at the start of the race.

E-12-b The system to recharge the batteries required in E-8 must be operational. It can use either wind, water, sun or fuel power.

E-12-c Generators must be equipped with an exhaust outside the cabin or must be usable outside the cabin.

E-12-d The auxiliary engine of the boat may be used to provide electricity power aboard if the propeller shaft has been sealed.

E-13 - ELECTRONIC EQUIPMENT

In addition to safety equipment (see E-8), only the following electronic equipment are allowed : autopilot, radar detector, battery tester, water/air temperature gauge, calculator, alarms, speed log, depth sounder, electronic barometer, positioning devices (GPS, Satnav, Decca etc...) without maps or plotters or communication facility allowing messages transmission and/or reception. Music players and video camera are allowed. Anemometer windvane systems are allowed.



E-14 - PROPULSION

Reminder of the RCV rule n° 42 : a boat shall compete by using only the wind and water to increase, maintain or decrease her speed. Her crew may adjust the trim of sails and hull, and perform other acts of seamanship, but shall not otherwise move their bodies to propel the boat.

E-15 - BOWSPRITS

Bowsprits and their associated hardware must stay folded on starboard within the beam limit of the boat (3 metres maximum) during the race start procedure.

E-16 - DRINKING RESERVES

E-16-a Mandatory drinking water reserves.

E-16-a-1 Each boat must carry a drinking water reserve of 3 litres per person, per 100 NM of each leg.

E-16-a-2 This water reserve shall be contained completely in one or more containers of 5 to 20 litres.

E-16-b Optional drinking reserves.

E-16-b-1 Each boat can have a reserve of drinks which volume is fixed to 2 litre per crew per 100 NM of each leg or race.

E-16-b-2 These reserves must be packaged in accordance with the summary table below.

	Mandatory water reserves	Optional drinking reserves
1/2 to 2 litres bottles	0	1 litre/100 NM
5 to 20 litres containers	3 litres/100 NM	1 litre/100 NM

E-16-c The containers quoted in articles E-16-a-2 and E-16-b-2 of a capacity superior or equal to 5 litres must be strong enough to resist an impact inside the boat, and must be hermetically closed and reusable.



E-16-d The maximum number of containers authorised at the start will be in direct proportion to the quantity of water defined by E-16-a and E-16-b. The Race Committee of each event must define for every leg the maximum quantity of drinks allowed on board.

E-16-e Filling and refilling the containers is permitted at sea.

E-16-f Emergency water-makers are allowed provided they are disclosed to the Race Committee and sealed.

Note : drinking water container quality is not measured for health - but containers labelled as "drinking water container" are often fragile whereas fuel containers are much more resistant and are fine for water when new.

E-17 - INSTALLATION OF EQUIPMENT

All heavy accessories and equipment (batteries, stove, liferaft, generator, anchor, water tanks, etc..) must be securely fastened so as to remain in position regardless of heel angle.

E-18 - BALLAST

Any kind of ballast is prohibited outside of the lifelines.

The RCV article n° 49.2 applies fully : lifelines shall be taut, and competitor can sit on the deck facing outboard, with his waist inside the lower lifeline and may have the upper part of his body outside the upper lifeline.



Mini Rules

2005 edition

The Mini Rules are designed to encourage offshore racing on small, moderately-priced monohull raceboats with short-handed crews.

The rules are intended to promote the research and development of security and performance of these sailboats in offshore racing.

Their function is also to help to develop seamanship, a sense of responsibility for the crews, and to promote sportsmanship and solidarity between competitors.

APPLICATION OF THE RULES

These rules are applicable from January 1st of the year, to all the Mini boats. The Administration Council of Classe Mini has full and exclusive authority to modify and interpret the rules.

The French version of Mini Rules for 2005 shall prevail.

CLASS MEASUREMENT

Inspection meetings will be scheduled during the sailing season. It is incumbent upon each skipper to ask the Classe for the dates of the inspection meetings and take appointment for one of them.

All non-measured boats or already measured but modified on any control point or effecting the measurements must be inspected at one of these meetings.

To participate in an official event published in the calendar, every boat must present a valid measurement form.



The cost of measurement is € 100 for a boat the Classe Mini does not know and € 60 for boats already measured but with a modification which requires a new measurement.

A measurement certificate will be provided to the owner after this meeting. The skipper before the start of each Mini race must show this certificate. The presence of the skipper and a crewmember is required to assist the measurer.

The owners of boats already measured which do not need to be measured again must contact the Classe Mini to have this certificate.

It is the skipper's responsibility to verify that his boat always respect the measurement rules in all means. Sanctions will be taken if he doesn't.

PRESENTATION TO A MEASUREMENT SESSION

The skipper must take an appointment to the Classe Mini before the measurement session. He must be present at the appointment place and hour decided with the Classe Mini. As it is difficult to schedule the measurement sessions, the calendar really has to be respected.

Registration to a session will be valid once paid. An unjustified absence will result in the loss of the payment.

The boat must be empty except for the batteries and the liferaft, without fenders, halyards attached to the deck collar, spinnaker pole or bowsprit in position. The loose appendices must be in high position.

The rigging must be tighten enough to allow the stability test in good condition. If he doubts, the measurer can ask for a new setting or refuse to make this stability test. In that case, the skipper will have to present the boat to another session at his expenses.

Physical stops or straps that limit the movements of mobile appendages must be in position so that their sealing can be applied right after the end of the stability tests.



What has to be shown :

Documents :

- the form showing the distribution of the flotation volume,
- the ship's license or equivalent,
- for the boats with a swinging keel, a written certificate from the designer detailing the worst configurations of the keel (specifying the positions) and of the ballasts for the large and small angles stability tests. In the event one does not present this document or presents wrong datas, the setting of the real limiting positions during the measurement session will result in a € 50 penalty coming to be added to the cost of the measurement session.

The boat must be rigged and ready to sail with :

On the deck

- the definitive version of the mast and its rig, completed with its equipments,
- waterproof partner,
- the low part of the mast must be watertight internally if the mast goes through the deck,
- air pipes of the ballast and their closing cap in position and fastened,
- the identification number of the boat on the deck and on the hull,
- the hatch boards for the companionway with the locking system that can be operated from inside and outside of the boat,
- lifelines in position,
- mainsail sheet in position.

Inside the boat

The skipper must ensure that there is no more water in the bilges and that all the personal or safety equipment is no more aboard except the following elements.

The following elements must be in position for the tests :

- the crash box bulkhead with inspection hatch if there is,



- the flotation reserves in position and fixed definitively,
- the ballasts in their final version with the pump and the equipment to bail, fill, isolate, empty or transfer,
- the drying equipment with two fixed pumps, one operable outside, the other operable inside, with the pipes,
- batteries fixed in their position,
- the protection of all through-deck bolts,
- the net and the electric equipment in position.

The Liferaft

According to the installation, the liferaft must be :

- in its final position inside the boat, which is less than one meter far from the transom, letting the emergency hatch accessible,
- in its final position outside the boat, on the border of the transom,
- the strap which inflates the liferaft must be fixed properly on the boat.

Once the visual checks and the measures are over, the measurer will proceed to the stability tests, with a 45 kg + 3 kg weight (including the Archimede's effect) at masthead.

As the test is stressing for the rigging, it is the skipper responsibility to chose which halyard to use to make this test. Either a mast head halyard or a fractional one.

HULL - ARCHITECTURE

A monohull is defined as a vessel having only one flotation plane, when stationary or with sails up in sailing conditions, in which the depth of the hull in all its sections must not decrease in the direction of the axis of symmetry.



J-1 - LENGTH

J-1-a The length of the hull must not exceed 6,50 metres.

J-1-b This dimension does not include rudders and their fittings, the bowsprit bobstay fittings (without devices intended to extend the water-line length) as well as the solar panels, pushpits, pulpits and windvane pilot.

J-1-c The length of the hull is measured between the perpendicular of the bow and transom.

J-2 - BOX (cf: figure 2 and 3)

J-2-a Measures are done in the measure trim specified in J-16-a except for the article J-2-c. The definition of the gunwale is in the figure 3.

J-2-b Max beam : 3 metres including mobile keel(s), rudders, daggerboard(s) and mobile mast(s).

J-2-c Max draft : 2 metres, regardless of heel angle or the position of the appendices or mobile appendices.

J-2-d From bottom of the keel to the top of the highest sail = 14 metres maximum (point showed if necessary by a 25 mm wide measurement mark, going around the mast profile).

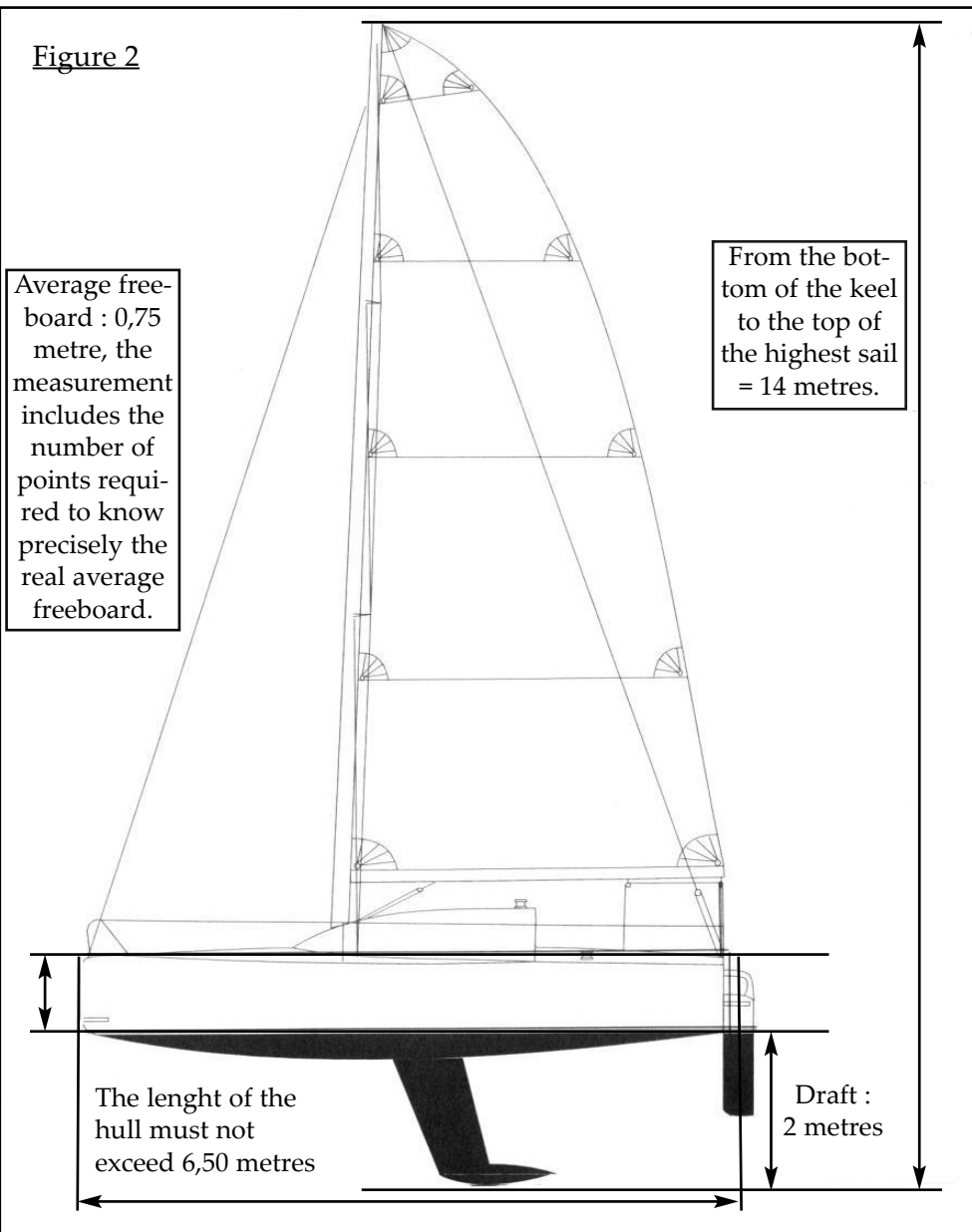
J-2-e Whatever their position, appendices (keel(s), daggerboard(s), rudders....) and mobile mast(s) (including spreaders) must not be out of the box for inexistant heel.

J-3 - AVERAGE FREEBOARD

Average freeboard : 0,75 metre at least at the gunwale level (the measurement of this will include the number of points required to know precisely the real average freeboard), except for boats built or boats that have been assigned by the Classe Mini before January, 1st, 1995.



Figure 2



J-4 - ROOF

J-4-a A roof is mandatory with a minimum volume calculated as follows :

$$\text{Roof Volume (m}^3\text{) minimum} = \frac{\text{Max deck beam in metres}}{6}$$

J-4-b The article J-4-a does not apply to boats built and that have been assigned by the Classe Mini before January 1st, 1995.

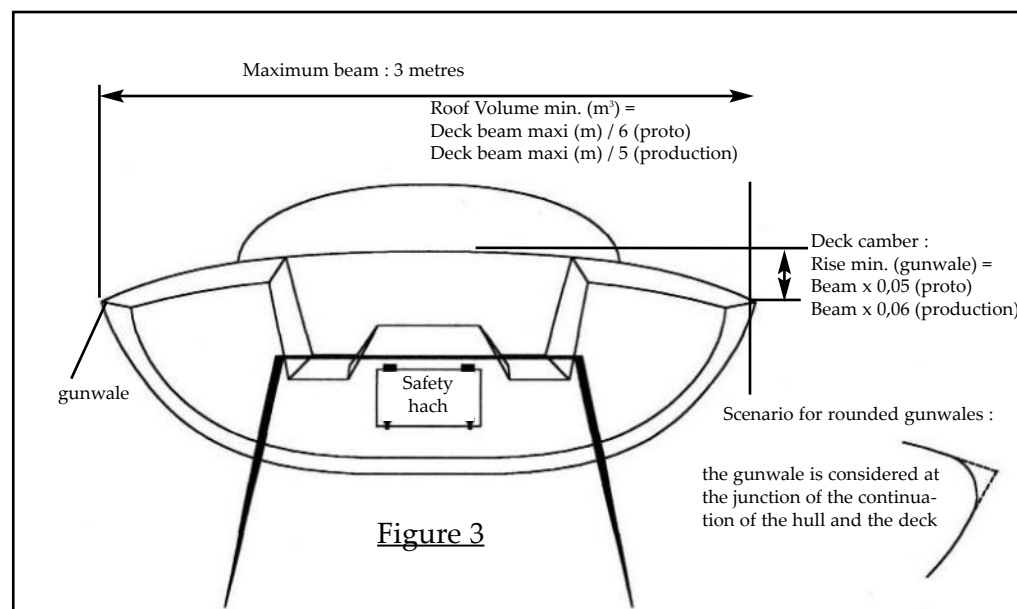


Figure 3

**J-5 - DECK CAMBER**

J-5-a Deck camber : minimum 5% of the beam at the gunwale level of the considered section, cf: figure 3.

J-5-b The article J-5-a does not apply to boats built and that have been assigned by the Classe Mini before January 1st, 1995.

J-6 - BULKHEAD

A watertight bulkhead must be located between 5% to 15% of LWL aft of the perpendicular of the bow.

J-7 - CONSTRUCTION

J-7-a The boats must be seaworthy and watertight.

J-7-b The standing rigging must be permanently fitted to the hull deck structure.

J-7-c The cockpit floor must be 15 cm minimum above the waterline.

J-7-d All through-deck bolts must be smoothed out and protected so as to prevent risks of injury.

J-7-e All boats must be equipped with one or more toerails, of a minimum height of 30 mm starting from the back foot of the pulpit and finishing at the level of the companion way or the roof cap.

J-8 - HULL AND DECK OPENINGS

J-8-a All hull openings (for ballast intake, outflow, etc..) must be equipped with seacocks and appropriately sized softwood plugs. The plugs for the speed log and depth sounder must be attached nearby.

J-8-b The hatch boards for the companionway and deck openings must be watertight.

J-8-b-1 They must be fixed to the boat with a system which cannot be freed without tools.



J-8-b-2 They must be equipped with a locking system that can be operated from inside and outside of the boat, acting on the same locking system.

J-9 - SAFETY HATCH

J-9-a A watertight safety hatch must be fitted on the transom (cf: figure 3), except for the boats built and having requested a race number to the Classe Mini before January, 1st, 1995.

J-9-b It must allow the skipper, liferaft and survival container to go through.

J-9-c This hatch must be above the waterline, regardless the position of the boat.

J-9-d It must be equipped with a locking system that can be operated from inside and outside of the boat, acting on the same locking system and with an outside handle.

J-9-e Watertightness of the hatch must not be obtained by sticking, filling or in a general way, by a system that would slow down or minimise the opening facility.

J-10 - MATERIALS

J-10-a Materials with a density greater than lead are prohibited.

J-10-b Titanium is forbidden.

J-11 - PULPIT, PUSH PIT, LIFELINES AND STANCHIONS

J-11-a A minimum of two stanchions per side, one pulpit and one or two pushpits are required.

J-11-a-1 The pulpit, pushpit and stanchions must be welded, bolted or bonded through the deck.

J-11-a-2 The pulpit must have a minimum of three points of fixation to the deck.



J-11-a-3 Each pushpit must have a minimum of three points of fixation to the deck on each side of the hull.

J-11-a-4 The stanchions must have a supporting leg orientated towards the inside of the boat.

J-11-a-5 Authorised materials : stainless steel and aluminium except for boats measured before January 1st, 2002.

J-11-b The boats must be equipped with two lifelines.

J-11-b-1 The upper lifeline must go round the entire boat.

J-11-b-2 The lifelines and their fixations must be able to resist, without break nor arrachement, to a longitudinal traction of 1100 kg. Belts of comfort can replace the lifelines if they are of an equivalent resistance and if the rings of extremity are forged or manufactured and not welded. Compulsory capacities : a sleeving for UV sensitive materials and a protection against the abrasion in sensitive spots for textiles.

J-11-b-3 The height between the upper lifeline and the deck must be between 45 and 60 centimetres.

J-11-b-4 The lower lifeline must be halfway between the deck and the upper lifeline.

J-11-b-5 The above rules are not applicable to boats built or boats that have been assigned by the Classe Mini before March, 15th, 2000, on the following point : J-11-b-3 and J-11-b-4. In that case, the 1999 text is valid "the height between the lower lifeline and the deck must be between 20 and 25 centimetres. The height between the upper lifeline and the deck must be between 40 and 60 centimetres".

J-11-b-6 The lifeline must go through or be permanently fixed to the stanchions and end at the pushpit and pulpit must be at the heights above.

J-11-b-7 The lashings at the ends must have a maximum length of 10 cm.

J-11-b-8 Webbing may not be used instead of steel cable lifelines.



J-11-c Boats must be equipped with jackstays on each side of the boat. They must be close enough to hook the harness while steering.

J-11-c-1 Jackstays and their fixations must be able to resist, without break nor arrachement, to a transverse traction of 1100 kg in each of their point. Compulsory capacities : a sleeving for UV sensitive materials and a protection against the abrasion in sensitive spots for textiles.

J-11-c-2 They are secured to points bolted or welded or bonded to the deck.

J-11-c-3 All boats must also have a fixed point near the hatchway to clip on with a harness.

J-11-d Boats have to have a system allowing the ascent aboard of a person fallen in the water. In case of doubt, the measurer can ask for a demonstration.

J-12 - WATER BALLASTS

J-12-a Water ballasts are allowed if they are fixed, located inside the hull and part of the structure of the boat.

J-12-b The total volume of water ballast must not exceed 400 litres symmetrically placed along the longitudinal axis of the boat.

J-12-c The vents for each ballast must be located on the deck and have a minimum inside diameter of 25 millimetres.

J-12-d Each vent must have a cap permanently tied close by.

J-13 - FLOTATION

J-13-a Each boat must be constructed with flotation volumes.

J-13-b They must be made of closed cell foam (absorption capacity less than 5%).

J-13-c They must ensure positive buoyancy of the boat under normal trim with a crew of 2 persons (equivalent to a minimum weight of 150 kg) in the event of accidental flooding.



J-13-c-1 The total flotation volume cannot under any circumstances be less than 1200 litres, spread over 4 volumes.

J-13-c-2 The volume of foam inserted in the building of the hull (exclusive of the deck) can be included for the required flotation volume.

J-13-d The flotation volumes must be either glued, bonded or partitioned off, or strapped up to resist loads of at least the value of their buoyancy.

J-13-d-1 The loads must be transferred back to the structure only (i.e. never to the cabin top or deck), regardless of the heel angle of the boat.

J-14 - APPENDAGES

J-14-a Mobile appendages must be operational when sailing.

J-14-b If the manoeuvres are operated outside, the water entries around the ropes must be limited with a rubber seal.

J-14-c Stops can be put for mobile appendages according to the stability rule (see J-16). These stops can be physical (stratification, glued or stratified shims....) or made with cable or textile straps.

Their making up must be coherent (resistance to the stress), this will be checked at the start of the races and they can be sealed.

J-15 - KEEL BULB

The height and width of the keel bulb must not exceed 450 mm.

J-16 - STABILITY

J-16-a The boat will be tested empty, excluding the following inventory :

- batteries fixed in normal position,
- liferaft in normal position,
- fixed weights :
 - ♦ fixed deck gear,
 - ♦ canting keel system,



- ♦ bilge pump system,
- ♦ fixed electrical and electronic system,
- ♦ pulpit, pushpit, lifelines, stanchions and jackstays,
- ♦ ballast and its devices,

- mobile weights :

- ♦ rudders, steering system, boom in the axis,
- ♦ halyards at the mast foot for one end, in their usual place for the other end,
- ♦ tight runners and backstays with their tackles,
- ♦ lifting daggerboards and rudders in up position.

J-16-b Large angles stability

J-16-b-1 The boat must not have water flooding.

J-16-b-2 It must have positive stability with a 45 kg weight (not included the Archimede's effect) at the top of the mast (measurement mark of the highest sail) in the most unfavourable ballasts, mobile keel and mast(s) configuration and all hatches open.

J-16-b-3 This test must be carried out with measurement mark at sea level.

J-16-c Small angles stability

The boat must not exceed a 10 degree heel angle in the most unfavourable ballasts, mobile keel and mast configuration.

J-17 - BILGE PUMPS

J-17-a Each boat must be equipped with two fixed bilge hand pumps. One operable inside the boat and the other one from outside the boat. Their mobile handle must be fixed close to each one.

J-17-b These two pumps must be operable with all the hatches closed and the pump must discharge at a rate of at least 0,5 litre per stroke.

J-17-c These two pumps must be in accordance with the standard ISO 15083.



J-17-d When the water ballast pump must be used to dry, the change from ballast to bilge function must be done only by the action of valves. It is not permitted to mount pumps on the same hose.

J-18 - IDENTIFICATION

J-18-a The serial number of the boat will remain the same for life. The number is provided to each boat by the Classe Mini. It must be displayed distinctively on each side of the hull within the first 25% of the length of the hull (see R-18-d) and on the deck.

J-18-b Lettering must be 50 cm high and 10 cm wide.

RIG

J-19 - DECK COLLAR

J-19-a Masts going through the deck must be watertight internally from the mast foot up to the first halyard exit.

J-19-b A system must be fitted around the mast and the deck collar so as to provide a complete watertight seal.

J-20 - INSTALLATION OF THE RIG

J-20-a The forestays, the backstays, the shrouds (permanent or temporary) must be fixed inside the length of the hull.

J-20-b The luffs of non-hanked sails shall not be considered as stays if they can be hoisted or dropped simply using a halyard. The use of a hook or lashing and other system to fix it to the mast is forbidden.

J-21 - BOOM

The booms may not extend beyond the transom with the mainsail up at full hoist.



J-22 - BOWSPRIT AND SPINNAKER POLE

J-22-a The bowsprits must be quickly and easily removed whilst sailing, as well as all other components extending over the hull extension (except for the boom).

J-22-b A tolerance of 30 mm is allowed for fixed components.

J-22-c Any spar, sprit and pole, removable without the use of any tool, in a limit of a rectangle 50 cm wide and 1 metre long, centred on the vertical of the stem may extend over the hull extension during the starting procedure.

J-23 - MATERIALS FOR THE RIG

J-23-a Mast : all materials are authorised, except those prohibited in the article J-10.

J-23-b Lateral shrouds and the main forestay : all materials are authorised, except those prohibited in the article J-10.

SAILS

J-24 - SAILS

J-24-a The number of approved sails carried on board must not exceed eight (8). This must include a storm jib and storm sail.

J-24-a-1 A storm jib with a maximum area of 4 m², fluorescent colour (if manufactured after 03/15/2000). It must be possible to hank it, and reefable to 2,5 m² if necessary.

J-24-a-2 A trysail of 5 m² maximum area of a fluorescent colour and a minimum weight of 340 g/m².

J-24-b Double film sails count as 2 sails.



J-25 - AUTHORISED MATERIALS FOR THE SAILS

- mainsail : woven polyester, with one window of 0,2 m² maximum area,
- headsail : woven polyester and polyester film,
- spinnaker : nylon.

J-25-a Grandfather rule for materials : composites sails can be kept if they have been approved during a Mini Classe event before January, 1st, 1995.

J-26 - IDENTIFICATION ON SAILS

J-26-a The race number must be worn on each side of the mainsails, stormsails, jibs (or, failing one, the genoa) and storm-jibs.

J-26-b Additionally, the mainsail must also display the national letters and the logo of the Classe Mini, available from the Classe Mini. National letters must be above the race number and starboard letters above port-side's.

J-26-c Dimensions of the numbers and letters are defined by the International ISAF rules, that is to say :

- height for figures and letters : at least 300 mm,
- space between figures or letters : at least 60 mm.



Production Boats Mini Rules

2005 edition

DEFINITION

A production boat is a boat allowing participation in Mini events as well as cruising. The price also must be cheaper than a prototype. It must be simple to use and accessible to inexperienced sailors.

SPECIFICS OF THE PRODUCTION BOAT MINI RULES

Production boats must be in accordance with Mini Rules and Production Boats Mini Rules. In the event of a conflict between the two rules, the specifications of the Production Boats Mini Rules shall prevail.

S-1 - AUTHORISED MATERIALS

S-1-a Hull : glass monolithic / polyester or plywood (minimum density of 0,4) / epoxy.

Production like prepreg are forbidden.

S-1-b Deck : balsa or foam sandwich, glass / polyester or glass monolithic glass / polyester or plywood / epoxy.

Production like prepreg are forbidden.

S-1-c Deck and hull reinforcements, deck and hull structure : only the authorised materials are allowed to use for the hull and the deck. Plain wood is authorised for the structure.

S-1-d Rudders : be careful, these are the forbidden materials : epoxy resin (core foaming epoxy is authorised), carbon fibre, kevlar, titanium. All the other materials are authorised.



S-1-e Keel fin : cast steel (any surfacing material is authorised, they cannot either take part of the keel structure, nor exceed 10 mm thick at any point). A reasonable margin can be accepted at about the junction to the edge.

S-1-f Keel bulb : lead or cast steel (any surfacing material is authorised).

S-1-g Pulpit, pushpit and stanchions : stainless steel.

S-1-h Chain plates for runners, backstays, forestays and shrouds : stainless steel or polyester glass if they are part of the deck's or hull's mould.

S-1-i Mast

- tube : it must be made of extruded aluminium with a weight of 2,1 kg/m minimum. Tapering is allowed above the forestay hound fitting,
- parts and reinforcements : aluminium or stainless steel,
- standing rigging (including forestay, lowerstay, any lateral shroud, backstay, runner and lower runner) : stainless steel cable except rod and Dyform cable,
- lashings are forbidden for the standing rigging.

S-1-j Other spars : aluminium is the only authorised material for tubes, except for the system B bowsprits specified in the article S-3-b-2.

S-2 - BOX AND SPECIFIC MEASURES

S-2-a Measures are done in the measure trim specified in J-16-a.

S-2-b Draft : 1,60 m maximum.

S-2-c From bottom of keel to the top of the highest sail = 12,60 metres maximum (point showed by a 25 mm wide measurement mark, going around the mast profile).

S-2-d Average freeboard at the gunwale level : 0,80 metre at least (the measurement of this will include the number of points required to know precisely the real average freeboard).



S-2-e Mandatory roof. The minimum volume must be of as calculated as follows :

$\text{Roof Volume (m}^3\text{)} = \frac{\text{Max deck beam in metres}}{5}$
--

S-2-f Deck camber : minimum 6 % of the beam at the gunwale level of the considered section.

S-2-g Minimum headroom : 1,40 metre at the highest point.

S-3 - VARIOUS

S-3-a The boom must stop at least 25 cm before the transom, mainsail up.

S-3-b One of three different types of spars can be used to establish spinakers and gennakers :

S-3-b-1 System A : spinnaker pole

Maximum length, including the flanges : 4 metres.

The spinnaker pole can only be anchored on the mast or a partner situated less than 10 centimetres from the mast.

On the deck, the pole must be anchored with at least two fixed points, one at the bow and the other at least 80 cm behind on the deck. For the back anchorage, only three positions are allowed : one in the axis, one on starboard and one on port side. The pole must not exceed 2,40 m in front the bow.

S-3-b-2 System B : removable bowsprit

Installation : sliding bowsprit (any other move excluded) and removable entirely under the deck.

Authorised materials for the tube : aluminium or fibre (including carbon fibre).



Maximum overall extension : 2,70 m over the bow. (Point showed by a measurement mark). In this position, the pole must be in the axis of the boat.

The bowsprit must be operable from the cockpit to deploy it in or out.

Watertight : the extraction system inside the boat must be totally and durably watertight.

S-3-b-3 System C : rotative bowsprit
The article J-22 is valid for this system.

Installation : the rotation must occur on the bow and must insure there is no interaction with the forestay. The bowsprit in folded position must not be outside the line of the gunwale.

Maximum length from the rotation axis to the tube end : 2,40 metres.

S-3-c Side arms and bobstays are allowed. Outriggers and poles can only be made of aluminium. The article J-22-c is valid.

S-3-d The shipyard can offer in option one or more system specified in S-3-b for the same production boat.

S-3-e Only one system specified in S-3-b can be set up on one boat.

S-3-f The system set up cannot be changed unless the skipper changes. Such a change must be done in conformity with the shipyard specification.

S-3-g Maximum number of bowsprit or spinnaker pole authorised on board : two of the same system.

S-3-h Water ballasts, kanting keels and centreboards are forbidden.

S-3-i A covered battery box must be designed in the bilges.

S-3-j Grandfather rule : the existing production boats, ratified by the Classe Mini before January, 1995 do not have to respect the present rule.

On the other hand, they have to respect the following points : S-3-b, S-3-c, S-3-d, S-3-e, S-3-f, S-3-g, S-3-h, S-3-i, S-3-j.



The boats must be in accordance with the ratification file the project manager provided.

S-4 - RATIFICATION

S-4-a All the boats of the same production must be built in the same professional shipyard, on the basis of a female mould (glass monolithic) or a manikin (plywood).

The construction of the boat can only be under the shipyard's responsibility. However, the owner can take part in the build in the shipyard (under the latter's control) but cannot build her entirely.

S-4-b The minimum configuration to deliver a boat is : structured hull with deck, pre-keeled. Appendices can be delivered separately but must be finished, ready to install.

S-4-c Samples of the various points must be the same for all the boats.

S-4-d The geometry of the various parts of the boat must be strictly the same (except the sails).

S-4-e Concerning the standing rigging, the boom, the spinnaker pole, the bowsprit and the mast, boats must be in accordance with the nomenclature or the measurement book the project manager provided.

S-4-f The project manager : architect and/or shipyard and/or class association must define a monotype rule for the following points :

- hull, deck and structure,
- appendices,
- sails,
- standing rigging,
- inside fittings,
- fixings, deck plan,
- authorised system(s) for spinnaker pole or bowsprit.

The monotype rule will help the project manager to settle the measurement book and will complete the actual production boats Mini rule.



Controls regarding the respect of this monotype rule can be done by the class or owners' association under the Classe Mini's responsibility.

S-4-g A ratification file must be sent to the Classe Mini, which is the only one entitled to let a new boat race according to the ongoing Production Boat Mini Rules.

S-4-h The boat will be declared Production boat once :

- the ratification file specified in S-5 is sent to the Classe Mini and the Measure Commission proposes the file to the Administrators' council that validates the boat,
- the project manager has built 10 strictly identical boats,
- the Classe Mini and the "jaugeur" have visited the shipyard (at the shipyard's expense) in order to check up the conformity of the building methods,
- the same boat has sailed without major incident :
 - 2000 NM in races of the Mini program with at least two 500 NM leg non stop,
 - one the 1000 NM single handed course specified in the article E-10-c, non-stop.

If the shipyard offers the B system option for bowsprit, these qualifications must be done by a boat with this system.

S-5 - DOCUMENTS

S-5-a For the first boat :

- identification number for the boat
- attestation that the boat respects the European Rules regarding the stability, the freeboard and the buoyancy.
- typical rule for leisure production boat (customs).
- plans :
 - . sails plan,
 - . rigging(s)' plan and nomenclature,
 - . deck plan,



- . inside fittings plan,
- . hull and deck structure plan,
- . nomenclature of the material for the rudders,
- . keel plan,
- . plan for each bowsprit or spinnaker pole system the shipyard offers,
- . measure book.

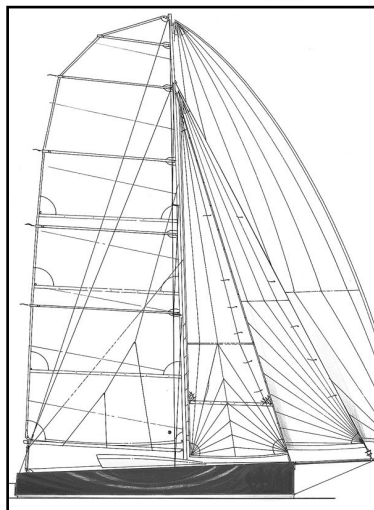
S-5-b For every one of the nine following boats :

- copy of the certificates of respect of the rule for leisure production boats,
- hull number,
- bowsprit and spinnaker pole system.



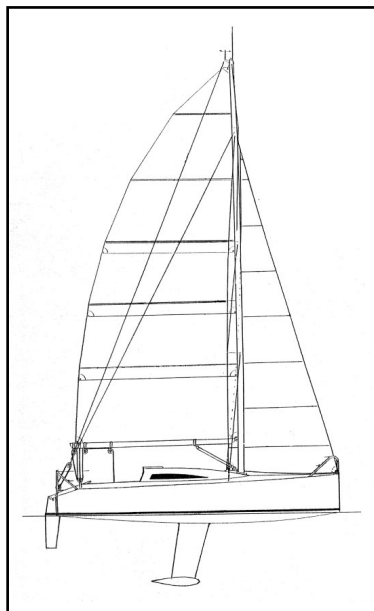
The prototypes

Since 1977, the prototypes have changed to become real race machines, over-sailed (until 50 m² upwind and 100 m² downwind) lighter and lighter and faster and faster. Designers and builders could explore new ways and thus develop a lot of technical innovations. The “Minis” have contributed to the adoption of the ballasts, the perfecting of the bowsprits equipped with genaker or asymmetrical spinnakers, the kites, the double-rudders and the kanging keels. Every year, a new generation of prototypes is being born in France and outside France, represented by more than 20 designers. More than 300 prototypes are listed at the Classe Mini in 2004.



Production boats

During the first Mini Transat, most of the boats were production boats, and in particular the famous “Muscadet” designed by Philippe Harlé with more than 300 built. But a lot of other production boats have competed in Mini events, sometimes adjusting them to the rules, such as : the “Surprise coupé” by Michel Joubert, the “Dufour T7”, the “Edel”, the “Serpentaire”... Starting from 1984, the “Coco” also designed by Philippe Harlé, has become the reference production boat with more than 80 boats sold as much in France than outside. Since 1995, the “Pogo” designed by Pierre Rolland has taken the place, it has been joined in 2000 by the Super Calin of Jean-Pierre Magnan and in 2003 by the Pogo 2 and the Naus 6,50. Other production boats are about to be born but 10 boats have to be built strictly in the same conditions so that the Classe Mini recognised it.



NOTES

